



CITY OF LA VISTA
PLANNING DIVISION

RECOMMENDATION REPORT

CASE NUMBERS: PPUD22-0003;

FOR HEARING OF: JUNE 20, 2024
REPORT PREPARED ON: JUNE 11, 2024

I. GENERAL INFORMATION

A. APPLICANT:

CPM-SFI Southport, LLC
Attn: Jamie Saldi
6910 N 102nd Circle
Omaha, NE 68122

B. PROPERTY OWNER:

CPM-SFI Southport, LLC
6910 N 102nd Circle
Omaha, NE 68122

C. LOCATION: Southeast of the intersection of Giles Road and Southport Parkway.

D. LEGAL DESCRIPTION: Lot 2 Southport East Replat Two.

E. REQUESTED ACTION(S): Approval of a Planned Unit Development Site Plan Amendment.

F. EXISTING ZONING AND LAND USE: C-3 – Highway Commercial/Office Park District, Gateway Corridor District (Overlay District), and Planned Unit Development; Lot 2 Southport East Replat Two is currently vacant, aside from some paved parking that currently services the adjacent Hampton Inn hotel.

G. PURPOSE OF REQUEST: Development including a fast-food restaurant with a drive-thru, and a commercial strip shopping center building.

H. SIZE OF SITE: Approximately 1.8 acres.

II. BACKGROUND INFORMATION

A. EXISTING CONDITION OF SITE: The property slopes slightly downward to the south and to the east; the site is currently vacant.

southeast, and through the entrance to the adjacent hotel parking lot off of S 123rd Plaza. McDermott Plaza is a private easement road with no dedicated public right-of-way.

2. An update to the 2022 Southport Lot 2 Replat 2 Traffic Impact Analysis for the development has been prepared by Felsburg, Holt & Ullevig (FHU), and suggests that the proposed site plan will adequately accommodate traffic demand for the proposed site development. The City Engineer has reviewed the analysis and finds it reasonable. FHU's update to the analysis, dated May 2, 2024, is attached.

D. UTILITIES:

1. The property has access to sanitary sewer, water, gas, power, and communication utilities.
2. Storm water management fees will be collected at the time of building permit issuance.

E. PARKING REQUIREMENTS:

1. The parking stall requirements for the proposed development, per the La Vista Zoning Ordinance, are:

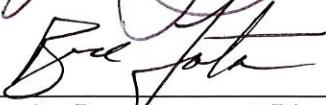
Lot #	Use	Required Stalls	Provided Stalls
Lot 2	Restaurant w/ Drive-Thru	18.2 (1 space per 150 Sq. Ft)	19
Lot 2	General Retail	21 (1 space per 200 Sq. Ft)	21
Lot 2	Restaurant	46 (1 space per 4 seats + 1 stall per employee on peak shift)	46

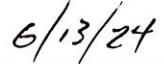
The amount of parking provided with the proposed development meets the amount required by the La Vista Zoning Ordinance. Additionally, there is a cross-parking agreement in place with the adjacent hotel, which provides for additional parking capacity.

F. LANDSCAPING:

1. The PUD Landscaping Plan has been reviewed by the City's Design Review Architect. Adjustments for each development within the PUD area will be reviewed as part of the overall building and site design package prior to the submittal of an application for building permit.


Prepared by: Deputy Community Development Director

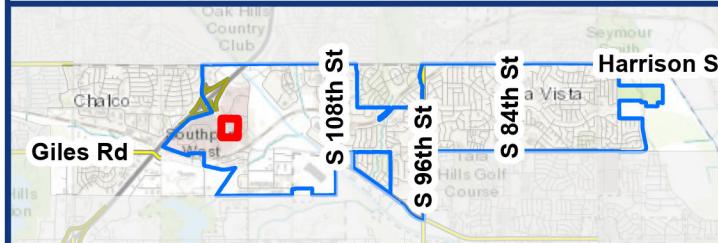

Community Development Director


6/13/24

Date



Vicinity Map - Lot 2 Southport East Replat 2 PUD Amendment



Legend

- Property Lines
- PUD Amendment Area





October 26, 2022

Jamie Saldi
CPM-SFI Southport, LLC
512 S 91st Avenue
Omaha, NE 68114

RE: Planned Unit Development Amendment – Initial Review Letter
Lot 2 Southport East Replat Two

Mr. Saldi,

We have reviewed the documents submitted for the above-referenced application. Based on the elements for consideration set forth in the applicable sections of the Zoning Ordinance, the City has the following comments:

General Comments

1. Please submit a proposed schedule of construction, per Section 5.15.04.01. If project phasing is planned, a phasing plan and schedule needs to be included. Since there are individual buildings that might be constructed separately, there needs to be information on the phasing of construction so that common area elements such as drives, storm water facilities, utilities or other common area shared items can be addressed for maintenance. If appurtenances are not all built with first phase/first lot, then financial guarantees to assure completion will be needed as part of PUD approval.
2. Per Section 5.15.04.06, please provide evidence of meeting and coordination with the hotel property owner regarding the PUD Site Plan and existing cross-parking easement. Any ingress/egress easements will need to be confirmed and/or recorded, and should be noted in the submittal.

City Hall
8116 Park View Blvd.
La Vista, NE 68128-2198
402.331.4343 P
402.331.4375 F

Community Development
8116 Park View Blvd.
402.593.6400 P
402.593.6445 F

Library
9110 Giles Rd.
402.537.3900 P
402.537.3902 F

Police
7701 S. 96th St.
402.331.1582 P
402.331.7210 F

Public Works
9900 Portal Rd.
402.331.8927 P
402.331.1051 F

Recreation
8116 Park View Blvd.
402.331.3455 P
402.331.0299 F

3. Per Section 5.15.09, amendments to an approved PUD plan and/or ordinance require approval from fifty-one (51) percent of the owners of property within the PUD district. As this amendment request is for a PUD district that contains just two properties, and subsequently, two owners, application approval must be provided by both property owners. The signed application constitutes your approval. Please provide a letter from the ownership of the adjacent property (Lot 1 Southport East Replat Two), that demonstrates their approval of your PUD amendment application, in order to comply with Section 5.15.09.
4. The building design for each of the proposed buildings must be reviewed as part of the design review process that is required for developments within Southport East and the Gateway Corridor District prior to building permit approval. The design review process will be conducted outside of the PUD approval process, with the exception of the review of the grading plan and landscaping plan.
5. Per Section D.ii.a.6 of the Southport East Design Guidelines, all light fixtures will need to meet the requirements of the Southport East Design Guidelines. Parking lot fixtures will need to conform to Appendix I of the Guidelines, whereas the corner feature light fixtures will need to conform to Appendix H of the Guidelines. Review of the proposed site lighting will be conducted at the time of the design review for the main structure(s).
6. Note that the development will need to obtain FAA approval prior to the issuance of a building permit, due to the close proximity of the Millard Airport.
7. Please confirm that the parcel is held in single ownership or control. If there are two or more owners, the application will need to be filled out jointly.
8. FHU study recommends pedestrian sidewalk improvements along the frontages of McDermott Plaza. Applicant should review applicable frontages to the site and confirm site connections to future and existing sidewalks are along McDermott Plaza are accounted for.
9. The development to the west, Southport West Replat 9 (please double check) is to cause sidewalk improvements to the ADA ramps and Pedestrian Push Buttons (PBB) as to enhance the conspicuity of the ADA ramps and install the PBBs at locations current with the 2010 ADA standards. The applicant should match the pedestrian improvement on the southeast corner of Southport Parkway and Giles Road as to set the companion ramp and PBB at current ADA standard.

10. A crosswalk connection from the southwest corner of the Hampton Inn to the sidewalk alongside the central parking area needs to be made. Please see the design changes required as per the 2020 iteration of the design for this development for details. ADA ramps for all sidewalk connections should be depicted.

11. Please submit a signage plan that identifies directional and wayfinding signage to try to minimize congestion and wrong-way movements.

12. Per Section 5.15.04.06, proposed site lighting locations need to be shown on the PUD Site Plan as well as the PUD Landscaping Plan.

13. The location of the dumpster enclosure for the southern-most building should be moved to the internal parking lot to reduce visibility from McDermott Plaza.

14. Per Section 7.06 of the La Vista Zoning Ordinance regarding the minimum off-street parking requirements, the required parking for your proposed site is 92 stalls (assuming that the number of employees on peak shift for the 5,700 square foot restaurant is 15) as outlined in the table below. The total number of parking stalls proposed in your plans for this property is 76, which results in a 16-stall deficit. Please confirm the number of anticipated employees on peak shift for the First Watch restaurant to confirm that this parking requirement is accurate. Please propose any adjustments necessary to this site plan to bring the proposed development into compliance with Section 7.06

Use	Square Feet	Requirement	Required Stalls
First Watch	5,700	1 stall / 4 seats (or 100 sq. ft.) + 1 stall per employee on peak shift	$57 + 15 = 72$
Scooters Coffee	664	1 stall / 150 sq. ft.	5
Chipotle	2,255	1 stall / 150 sq. ft.	15
Total			92

Sheet C1.0

15. Please submit all applicable easements and covenants with neighboring property owners, per Section 5.15.04.03.

16. Please include the heights of all proposed buildings on the project site, per Section 5.15.05.02.2.

17. Please update the legend showing the thickness of all P.C.C. pavement and sidewalks.
18. Please include calculations showing how stall count was determined and if parking stalls will be shared by proposed businesses, per Section 5.15.04.08.

Sheet C2.0

19. Please confirm that grading will not interfere with the drainage swale on the neighboring lot located south of the construction activity.

Sheet C3.0

20. The proposed number and location of storm inlets appear to be inadequate. Please provide further details on how the parking lot will drain, per Section 5.15.05.04.
21. Please show the point of connection and other pertinent design information for the sanitary sewer, per Section 5.15.05.04.
22. Please show the point of connection and other pertinent design information for the to the public stormwater infrastructure, per Section 5.15.05.04.
23. Please show locations of natural features such as ponds, tree clusters, and drainageways, per Section 5.15.05.02.10.
24. Please submit a post-construction stormwater management plan, inclusive of a drainage study. In addition, a NPDES grading permit will be required at time of building permit.

Sheet C4.0

25. Pertaining to Section 5.15.04.08 of the La Vista Zoning Ordinance, the proposed landscaping plan is currently under review by the City's third-party design review architect. Landscaping for the development will need to meet the requirements of the Southport East Design Guidelines. Review comments will be provided when they are available.
26. Not all utility boxes are depicted along Southport Parkway. Please depict all boxes in the landscaping plan and related vegetative screening. Access doors to boxes cannot be blocked.

27. The landscaping plan does not depict the existing Center Identification Sign for the subdivision. Please depict this sign and adjust the location of any landscaping, as necessary.
28. No signage is depicted within any of the plans in this PUD plan set. Proposed ground monument signage locations, including setbacks, need to be depicted and landscaping should be adjusted accordingly.
29. Please make any necessary adjustments to the Landscaping Plan that may result from the introduction of the light poles.
30. Berming along street frontages is a requirement as per the Southport East Design Guidelines. Berming should be reflected in the PUD Grading Plan and the PUD Landscaping plan.

Please submit 2 full size revised copies of the PUD plans and related documents (along with electronic copies) to the City for further review. A timeline for review by the Planning Commission and City Council will be determined based on the timing of the resubmittal and the extent to which the issues noted this review have been sufficiently addressed.

If you have any questions regarding these comments, please feel free to contact me at any time.

Thank you,



Christopher Solberg, AICP
Deputy Community Development Director
City of La Vista
csolberg@cityoflavista.org
(402) 593-6400

cc:

Doug Kellner, Thompson, Dreessen & Dorner, Inc.
Bruce Fountain, Community Development Director – City of La Vista
Cale Brodersen, Assistant City Planner – City of La Vista
Pat Dowse, City Engineer – City of La Vista



March 27, 2024

Jamie Saldi
CPM-SFI Southport, LLC
512 S 91st Avenue
Omaha, NE 68114

RE: Planned Unit Development Amendment – Second Review Letter
Lot 2 Southport East Replat Two

Mr. Saldi,

We have reviewed the documents submitted for the above-referenced application. Based on the elements for consideration set forth in the applicable sections of the Zoning Ordinance, the City has the following comments:

General Comments

1. Regarding your response to comment #1 on the initial review letter, the schedule of construction depicts a construction start date of 5/6/24, which does not include sufficient time for the PUD approval process, design review, and the subsequent building permit approval. However, the phasing notes listed on Sheet C1 appear to be more viable.
2. Regarding your response to comment #4 on the initial review letter, you mentioned that the PUD amendment and design review submittals are being submitted simultaneously. We have received an application form and the design review fee for the proposed Chipotle building, but none of the other submittal items. Without a complete submittal of the design review application for the Chipotle building, we will not be able to begin the review.

City Hall
8116 Park View Blvd.
La Vista, NE 68128-2198
402.331.4343 **P**
402.331.4375 **F**

Community Development
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402.593.6400 **P**
402.593.6445 **F**

Library
9110 Giles Rd.
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402.331.1582 **P**
402.331.7210 **F**

Public Works
9900 Portal Rd.
402.331.8927 **P**
402.331.1051 **F**

Recreation
8116 Park View Blvd.
402.331.3455 **P**
402.331.0299 **F**

Please reference the submittal checklist on the design review application. Pre-application meetings are also required for the design review process, so please reach out when you would like to schedule that pre-application meeting. It would be beneficial to include Chipotle's architect in this meeting to ensure they understand the requirements and process.

3. Regarding your response to comments #5 and #12, a full review of the site and building lighting will be conducted at the time of design review for each building. However, please note that three pedestrian-scale lighting poles/fixtures will need to be installed around the corner pedestrian plaza as per Appendix A of the Southport East Design Guidelines.
4. Regarding your response to comment #11 on the initial review letter, a sign that states "Not an Exit" or similar message shall be placed in addition to the directional arrow markings to limit driver attempts to exit onto Southport Parkway.
5. Regarding your response to comment #16 on the initial review letter, the PUD site plan must depict the building heights in call-outs or a table on the plan set. If the proposed building height for the First Watch and retail building is unknown at this time, please include a note that it is less than 90' (the existing height maximum for the C-3 zoning district) to demonstrate that it will comply with the requirements.
6. Regarding Section 5.15.01.04, please submit an updated traffic impact analysis based upon the traffic demands of the revised site plan. The previous TIA was analyzing a different mix of commercial uses.
7. The landscaping plan for the proposed development is currently under review by the City's third-party design review architect and comments may be forthcoming. Prior to building permit application submittal for either building proposed as part of this PUD amendment, the design for such building (and related site elements) must first be approved through the design review process.
8. Per Section 7.06 of the La Vista Zoning Ordinance regarding the minimum off-street parking requirements, the required parking for your proposed site is 105 stalls (assuming that the number of employees on peak shift for the 4,935 square foot restaurant is 15) as outlined in the table below. The total number of parking stalls proposed in your plans for this property is 86, which results in a 19-stall deficit. Please confirm the number of anticipated employees on peak shift for the First Watch restaurant to confirm that this parking requirement is accurate. Please

propose any adjustments necessary to this site plan to bring the proposed development into compliance with Section 7.06.

Use	Square Feet	Requirement	Required Stalls
First Watch	4,935	1 stall / 100 sq. ft. + 1 stall per employee on peak shift	$50 + 15 = 65$
General Sales	4,200	1 stall / 200 sq. ft.	21
Chipotle	2,728	1 stall / 150 sq. ft.	19
Total			105

Sheet C1.0

9. The PUD site plan identifies locations for two ground monument signs. Please include the setback dimensions for such signs in the site plan. The required setback for a ground monument sign (advertising a single tenant) is 10 feet. The northwestern-most sign is proposed to advertise both the First Watch restaurant and the general retail tenants, which would make this either a center ID sign or a project directory sign (see Section 7.01 of the La Vista Zoning Ordinance), which each require a 20-foot setback along arterial roads such as Giles Road. If you would like this sign to advertise more than one tenant, then it must be able to meet the 20-foot setback.

Sheet C3.0

10. Regarding Section 5.15.05.04, please be aware that manholes will be required at each change of alignment in the proposed sanitary sewer and that they must be accessible by maintenance vehicles at all times.
11. Please confirm the existence of the sanitary sewer where the proposed manhole connection is located.
12. Please provide for review a draft sanitary sewer easement in coordination with the property owner of Lot 1 Southport East Replat 2. A sanitary sewer easement will be required between Lots 1 and 2 for the conveyance of sanitary waste from Lot 2.

Please submit 2 full size revised copies of the PUD plans and related documents (along with electronic copies) to the City for further review. A timeline for review by the Planning Commission and City Council will be determined based on the timing of the resubmittal and the extent to which the issues noted this review have been sufficiently addressed.

If you have any questions regarding these comments, please feel free to contact me at any time.

Thank you,



Cale Brodersen, AICP
Associate City Planner
City of La Vista
cbrodersen@cityoflavista.org
(402) 593-6400

cc:

Doug Kellner, Thompson, Dreessen & Dorner, Inc.
Bruce Fountain, Community Development Director – City of La Vista
Chris Solberg, Deputy Community Development Director – City of La Vista
Pat Dowse, City Engineer – City of La Vista
Garrett Delgado, Engineering Assistant – City of La Vista

SOUTHPORT LODGING, LLC
1000 O Street, Ste. 102
LINCOLN, NE 68508
(402) 369-5211

Via Email

October 4, 2023

City of La Vista
Planning Department
8116 Park View Blvd
La Vista, NE 68128

RE: PUD/Site Plan Review
Lot 2 Southport East Replat Two

To Whom It May Concern:

This letter is to provide notice that Southport Lodging, LLC, as the owner of Lot 1 Southport East Replat Two, consent to the site plan attached to this letter and approve a PUD site plan amendment consistent with the attached site plan.

Please feel free to contact either of us with any questions.

Sincerely,

Southport Lodging, LLC, a Nebraska
limited liability company

By: 

Scott Brown, Manager

By: 

Mike Works, Manager

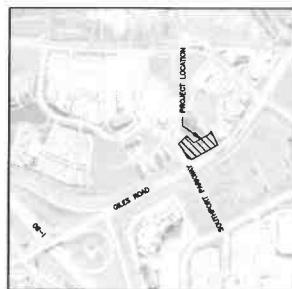
DocuSigned by:

Mike Works

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TD2

TICCO

Thompson, Dresbeck & Donner, Inc.
10636 Old Mill Rd
Columbia, MD 21046
p: 301.330.0860 w: www.ticco.comLot 2, Southport
East Replat TwoPlatinum Group
Development

VICINITY MAP

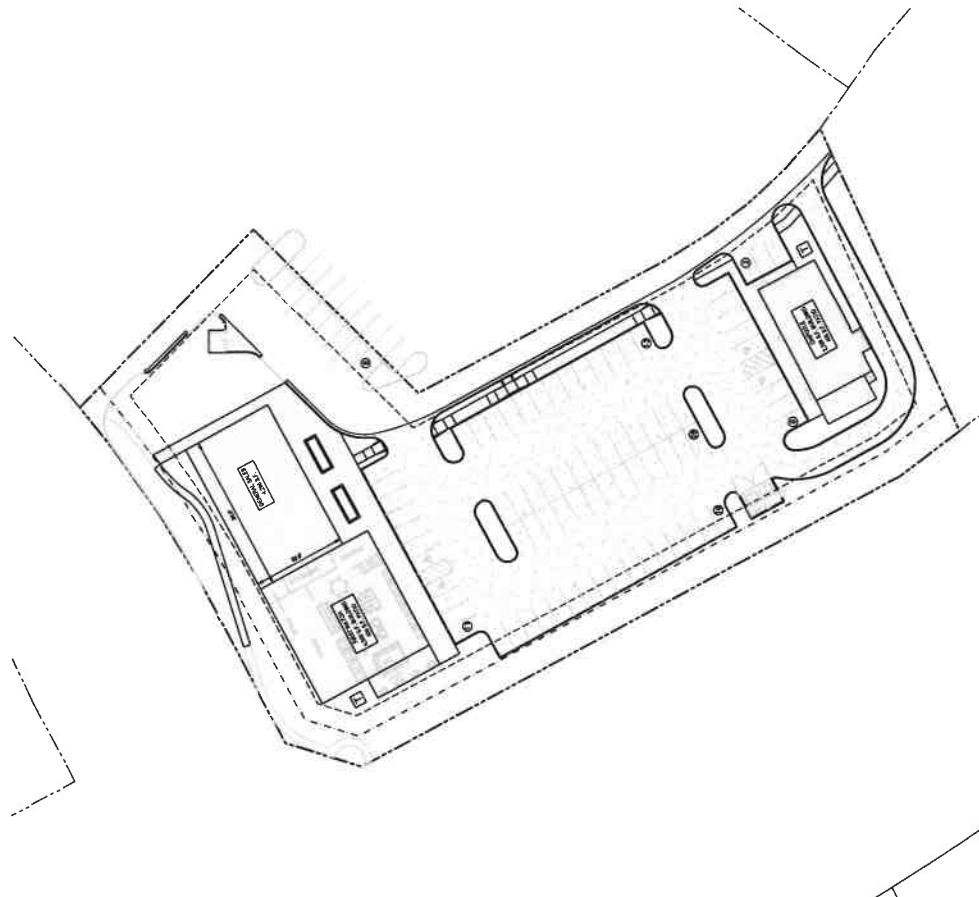
LEGEND

PROPOSED P.T.C. PHARMACY
PROPOSED P.C.C. PHARMACY
EXISTING P.C.C. PHARMACY
PROPOSED CAFE
BURGERS BY STEVE
LANDSCAPING SETBACK

PARKING SUMMARY	
STALL TYPE	STALLS
STANDARD STALLS	82
ADA STALLS	4
TOTAL PARKING	86

No.	Description	MANUFACTURER	
		1	2
1	ADA STALL		
2	STANDARD STALL		
3	STANDARD STALL		
4	STANDARD STALL		
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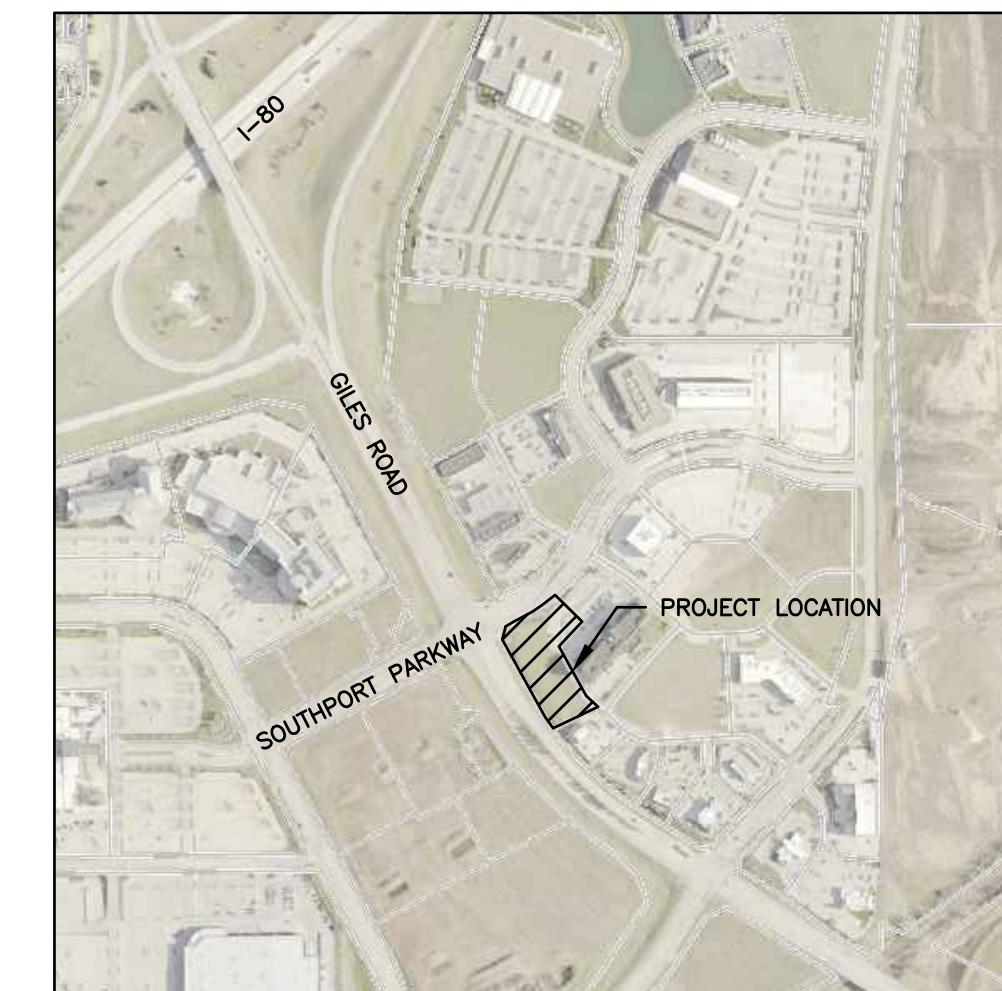


Reviewed By: AD

Date: 05/26/23

Job No.: 2259-151

Site Plan



VICINITY MAP

LEGEND

 PROPOSED 9" P.C.C. PAVEMENT WITH
INTERGRAL TYPE "A" CURB

 PROPOSED 7" P.C.C. PAVEMENT WITH
INTERGRAL TYPE "A" CURB

 PROPOSED 4" P.C.C. SIDEWALK
SIDEWALK IN R.O.W. SHALL BE 5" P.C.C.

10 PARKING COUNT

----- BUILDING SETBACK

— : — : — : — LANDSCAPING SETBACK

PARKING SUMMARY

PARKING SUMMARY	
STALL TYPE	STALLS
STANDARD STALLS	82
ADA STALLS	4
TOTAL PARKING	86

PARKING SUMMARY – REQUIRED

PARKING SUMMARY – REQUIRED					
USE	SF	REQUIREMENT	CALCULATION	REQUIRED STALLS	
RESTAURANT	4485	1 STALL / 4 SEATS + 1 STALL PER EMPLOYEE ON PEAK SHIFT, THROUGH PUD ORDINANCE ALLOWANCE	$(\frac{1 \text{ STALL}}{4 \text{ SEATS}}) * (136 \text{ SEATS}) + (\frac{1 \text{ STALL}}{1 \text{ EMPLOYEE}}) * (12 \text{ EMPLOYEES}) = 46 \text{ STALLS}$	46	
GENERAL RETAIL	4200	1 STALL / 200 SF	$(\frac{1 \text{ STALL}}{200 \text{ SF}}) * (4,200 \text{ SF}) = 21 \text{ STALLS}$	21	
CHIPOTLE	2325 + 403	1 STALL / 150 SF	$(\frac{1 \text{ STALL}}{150 \text{ SF}}) * (2,728 \text{ SF}) = 18.2 \text{ STALLS}$	19	
TOTAL PARKING					86

SITE STATISTICS

Site Statistics											
Lot	Zoning	Lot Size (sf/ac)	Max. Building Height (ft)	Building Coverage (sf)	Building Coverage (%)	Gross Floor (sf)	Pavement Area (sf)	Total Impervious (sf)	Total Impervious (%)	Open Space (sf)	Total Open Space (%)
1	C-3 (PUD) GATEWAY CORRIDOR DISTRICT	53,521 SF/ 1.229 AC	LESS THAN 90	8,685	16	8,685	30,337	39,022	73 %	14,499	27 %
2	C-3 (PUD) GATEWAY CORRIDOR DISTRICT	24,998 SF/ 0.574 AC	20.67	2,325	9	2,325	15,290	17,615	70 %	7,383	30 %

PHASING INFORMATION

PHASING INFORMATION		
PHASE NUMBER	DESCRIPTION	SCHEDULE
PHASE 1	<ul style="list-style-type: none"> - GRADING AND UTILITIES FOR LOT 2 - PAVING OF REQUIRED PARKING STALLS AND DRIVE ISLES - CONSTRUCTION OF THE CHIPOTLE BUILDING 	FALL 2024 – SPRING 2025
PHASE 2	<ul style="list-style-type: none"> - GRADING AND UTILITIES FOR LOT 1 - PAVING OF REMAINING PARKING STALLS AND DRIVE ISLES - CONSTRUCTION OF THE FIRST WATCH/ RETAIL BUILDING 	SUMMER 2025 – SPRING 2026

Drawn By: GTN Reviewed By: JAD
Job No.: 2259-151 Date: 05-06-24

Sheet Title

Site Plan

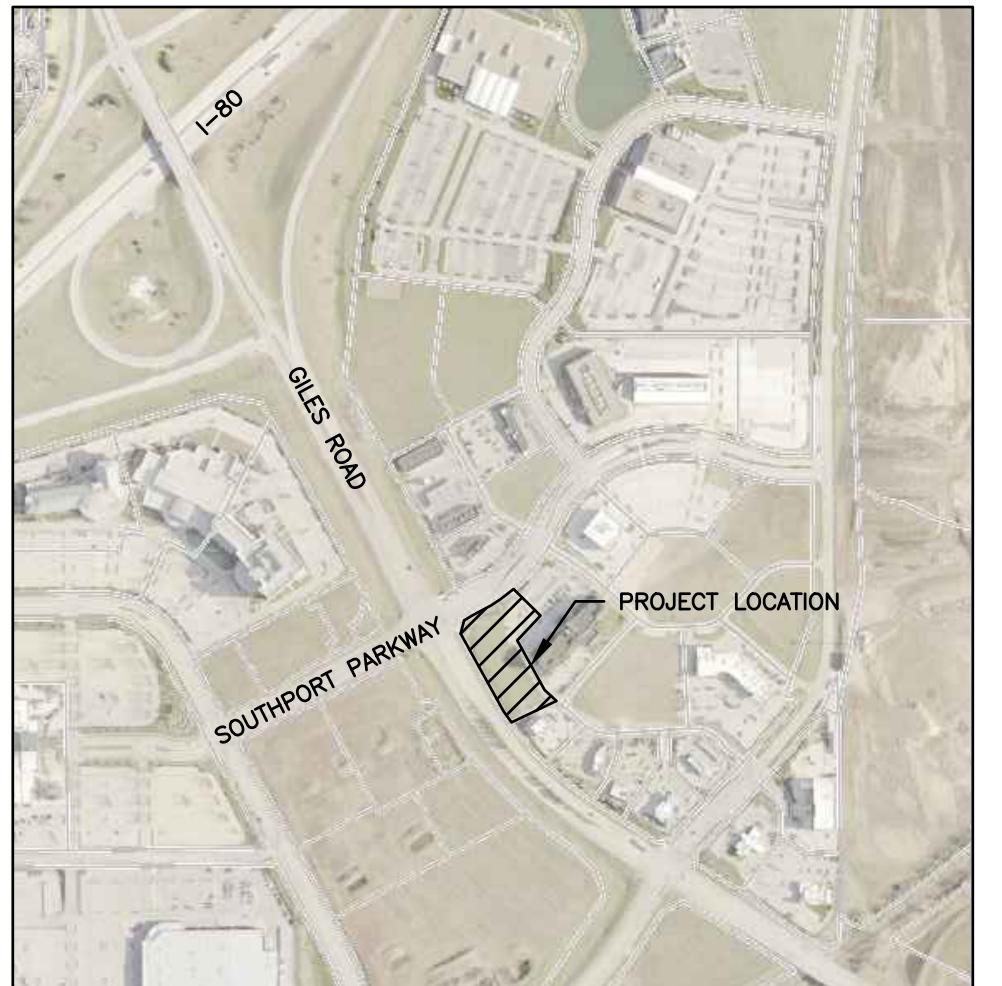
Site Plan

Sheet Number

c1.0

This architectural site plan illustrates the layout of Southport East Replat Sixteen, featuring the following key components and details:

- Southport Parkway:** A major road running diagonally across the site, with a "GENERAL SALES" building (4,200 S.F.) located along its edge.
- McDERMOTT PLAZA:** A central plaza area with a "CHIPOTLE" building (2,325 S.F. BUILDING, 403 S.F. PATIO) and a "RESTAURANT" building (4,485 S.F. BUILDING, 450 S.F. PATIO).
- Proposed Lots:**
 - Proposed Lot 1, Southport East Replat Sixteen:** Located on Giles Road, featuring a "FIRST WATCH/RETAIL MONUMENT SIGN".
 - Proposed Lot 2, Southport East Replat Sixteen:** Located on Giles Road, featuring a "CHIPOTLE MONUMENT SIGN".
 - Proposed Lot 16:** A large, irregularly shaped plot bounded by Southport Parkway, Giles Road, and McDermott Plaza, containing the "GENERAL SALES" and "RESTAURANT" buildings.
- Signage:** Various signs are indicated, including an "EXISTING 'NOT AN EXIT' SIGN" and "CHIPOTLE MONUMENT SIGN".
- Dimensions:** Numerous dimensions are provided for building footprints and setbacks, such as 25.0', 20.0', 15.0', 24.0', 24.5', 36.0', 18.0', 21.0', 18.0', 24.0', 25.0', 10.0', 10.4', and 27.0'.
- Lot Information:**
 - LOT 1:** Southport East Replat Two, owned by Lavista Lodging Investors LLC.
 - LOT 5:** Southport East Replat Six.
 - LOT 6:** Southport East Replat Six, owned by TNT Holdings LLC.



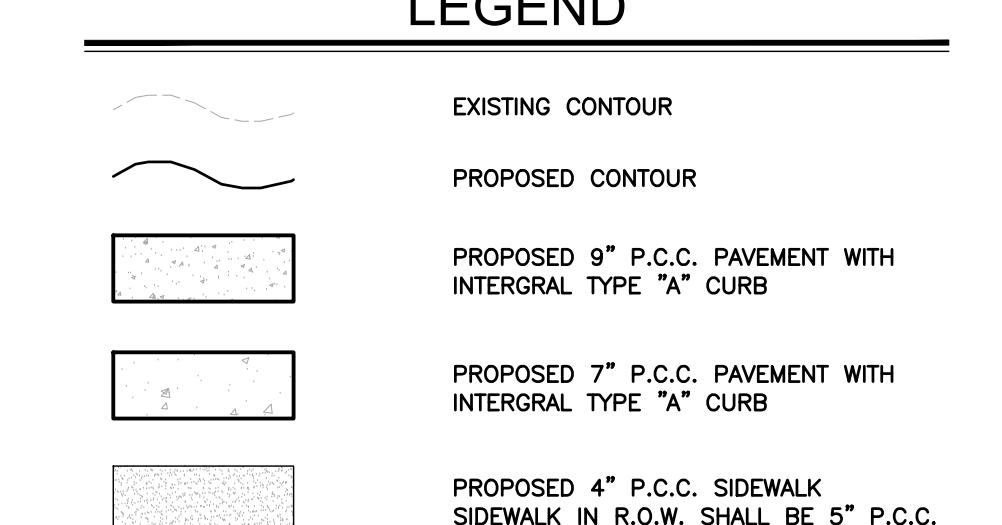
VICINITY MAP



Project Location

Southeast Corner of Southport Parkway and Giles Road

Platinum Group Development



Professional Seal

Revision Dates

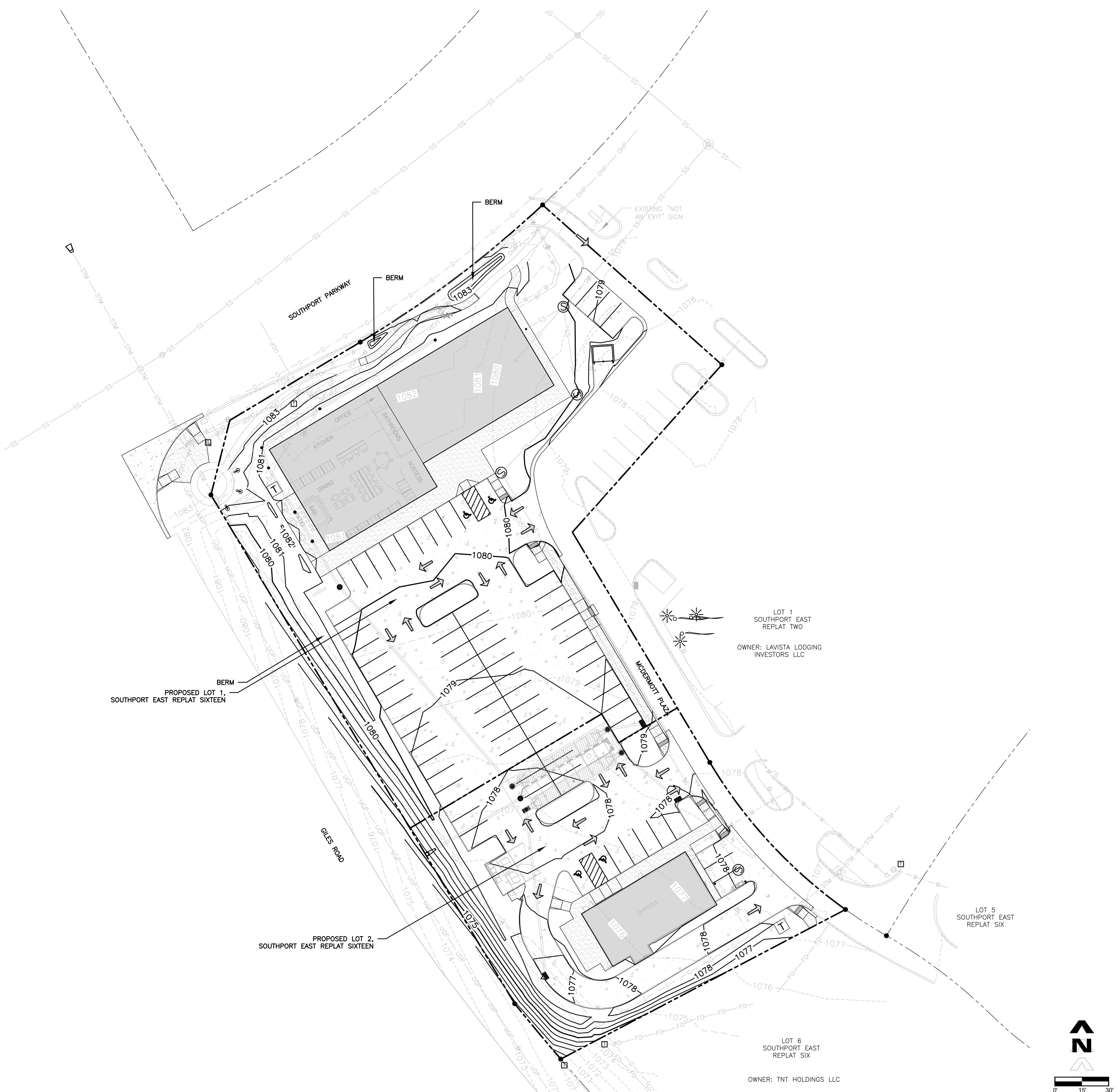
Drawn By: GTN Reviewed By: JAD
Job No.: 2259-151 Date: 05-06-24

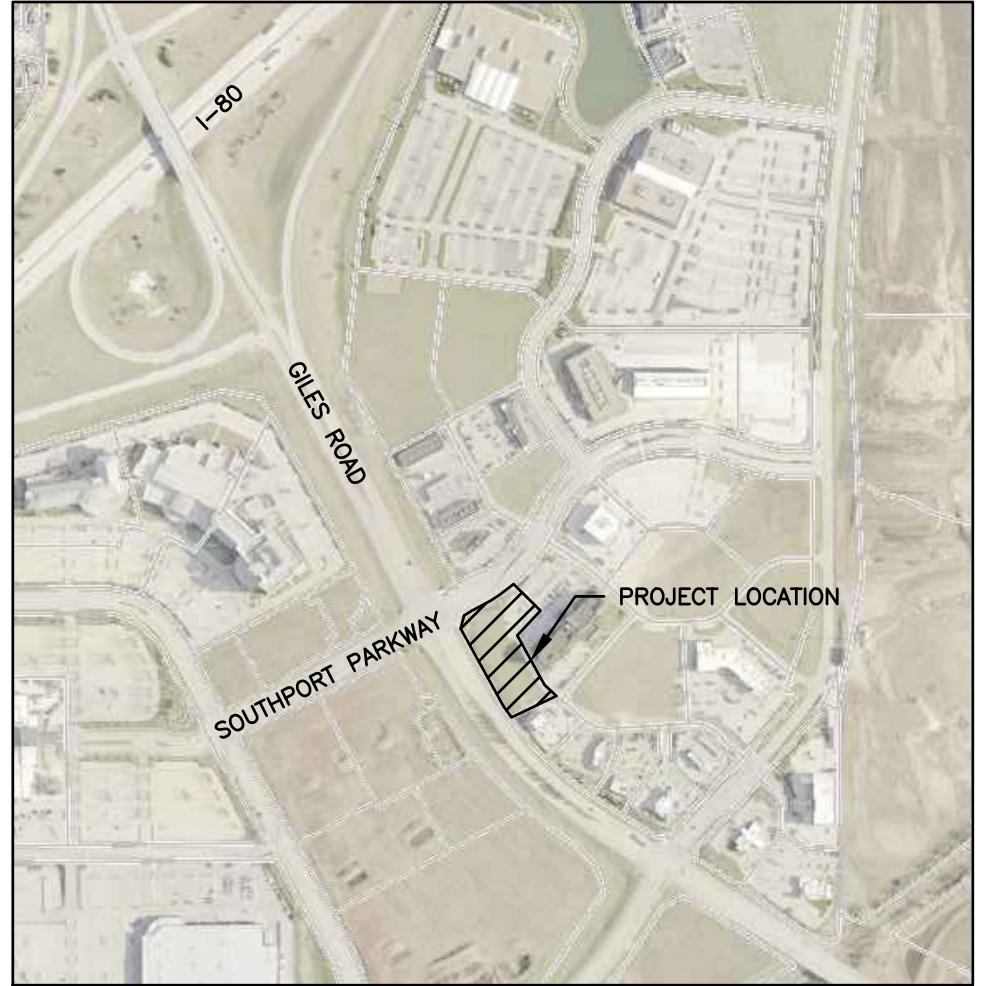
Sheet Title

Grading Plan

Sheet Number

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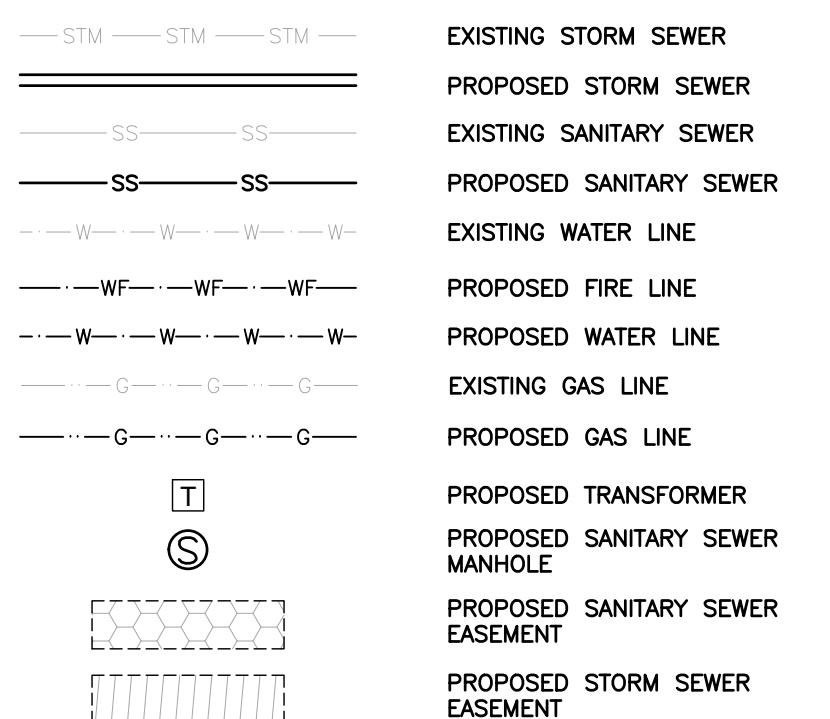


VICINITY MAP



Project Location

Platinum Group Development



LEGEND



Professional Seal

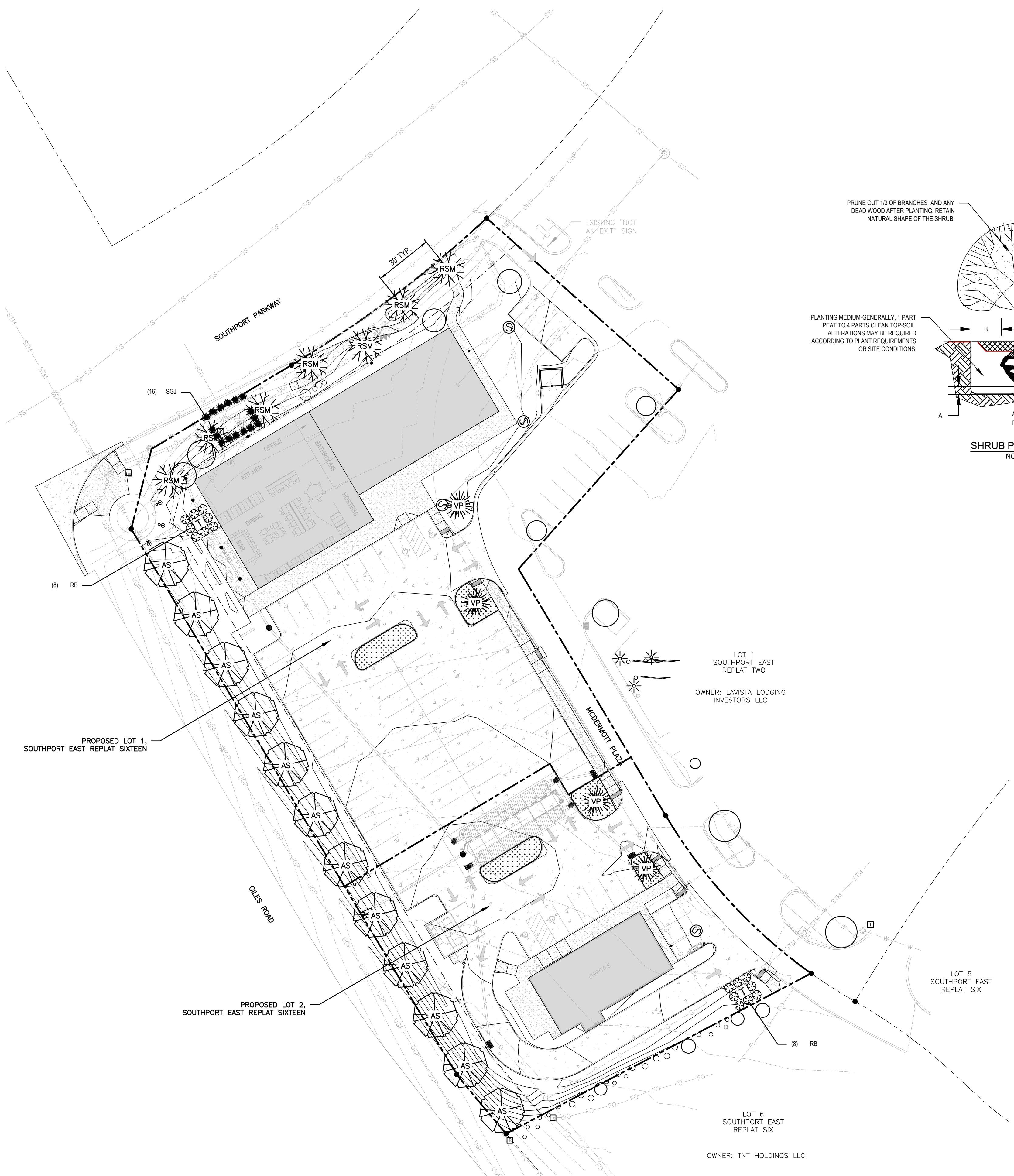
Revision Dates

Drawn By: GTN Reviewed By: JAD
Job No.: 2259-151 Date: 05-06-24

Utility Plan

Sheet Number

C3.0



LANDSCAPE CALCULATIONS

LEGEND

PROPOSED 9" P.C.C. PAVEMENT WITH
INTERGRAL TYPE "A" CURB

PROPOSED 7" P.C.C. PAVEMENT WITH
INTERGRAL TYPE "A" CURB

PROPOSED 4" P.C.C. SIDEWALK
SIDEWALK IN R.O.W. SHALL BE 5" P.C.C.

INTERIOR LANDSCAPING

LANDSCAPE SETBACK

EXISTING TREES

EXISTING SHRUBS

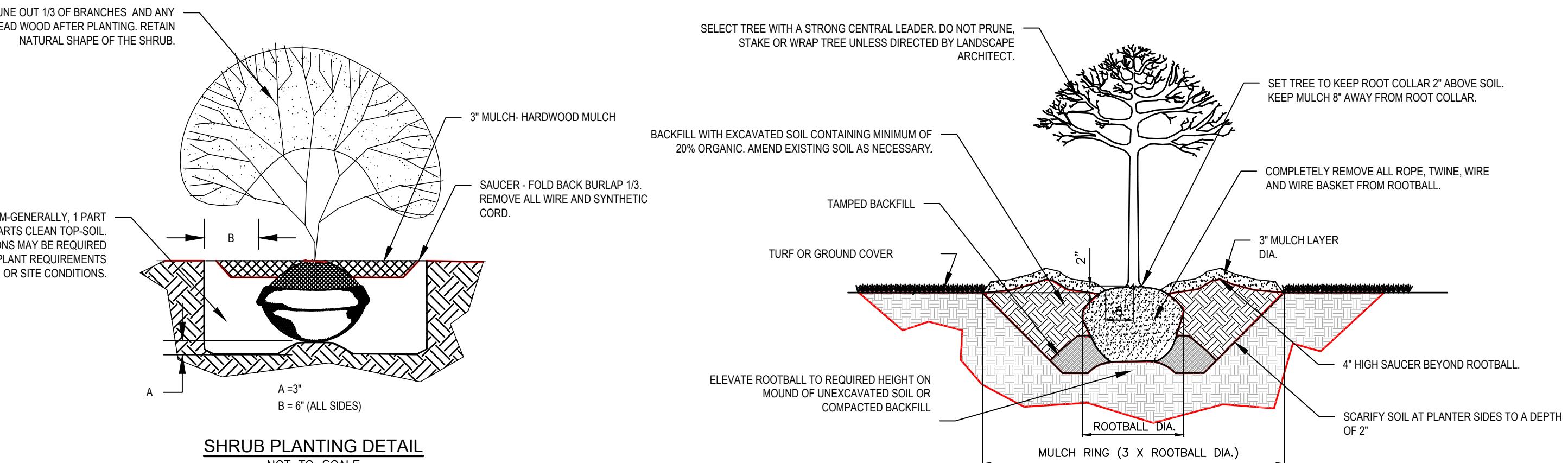
thompson, dreessen & dorner, inc.
10836 Old Mill Rd
Omaha, NE 68154
p.402.330.8860 www.td2co.com
dba: TD2 Engineering & Surveying
NE CA-0199

Project Name

Southport East

Project Location

Platinum Group Development



LANDSCAPE NOTES

1. ALL LANDSCAPED AREAS SHALL BE EQUIPPED WITH AN IRRIGATION SYSTEM.
2. ALL AREAS NOT SHOWN AS BUILDING, PCC PAVEMENT, OR SIDEWALK SHALL BE TURF TYPE FESCUE.
3. ALL LANDSCAPING MATERIALS & CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF LA VISTA'S LANDSCAPE ORDINANCES AND REGULATIONS.

- C) COMPLETE REMOVAL OF ALL WIRE, ROPE, AND TWINE.
- D) FINISH BACKFILL AND WATER.

3. STREET TREE PLANTING INSPECTION AND APPROVAL BY CITY OF LA VISTA AND THE OWNER SHALL BE DONE IN TWO STAGES:
 - A) EXCAVATION OF PLANTER AREA AND COMPACTION RELIEF TREATMENT PRIOR TO PLANTING AND BACKFILLING.
 - B) FINAL INSTALLATION AFTER PLANTING AND BACKFILLING.

TYPICAL TREE PLAN

PLANT SCHEDULE							
KEY	BOTANICAL NAME	COMMON NAME	SIZE	COND.	MATURE HEIGHT	MATURE SPREAD	TOTAL QUANTITY
DECIDUOUS TREES							
RSM	ACER RUBRUM 'FRANKSRED'	RED SUNSET MAPLE	3" CAL	B&B	45'	35'	7
AS	PLATANUS OCCIDENTALIS	AMERICAN SYCAMORE	3" CAL	B&B	75'	75'	12
CONIFEROUS TREES							
VP	VANDERWOLF PINE	PINUS FLEXILIS 'VANDERWOLF'S PYRAMID'	3" CAL	B&B	25'	15'	16
DECIDUOUS SHRUBS							
RB	BERBERIS THUNBERGII	RED BARBERRY	5 GAL	CONT	6'	4'	16
CONIFEROUS SHRUBS							
SGI	JUNIPERUS CHINENSIS 'SEA GREEN'	SEA GREEN JUNIPER	5 GAL	CONT	6'	4'	16

NOTE: PLANT SCHEDULE IS APPROXIMATE AND SUBJECT TO CHANGE; HOWEVER, ANY CHANGES MUST BE IN ACCORDANCE WITH THE CITY OF LA VISTA'S LANDSCAPE ORDINANCES AND REGULATIONS.

IRRIGATION PERFORMANCE SPECIFICATIONS

1. OWNER/ARCHITECT SHALL REVIEW AND APPROVE IRRIGATION CONTROLLER PRIOR TO INSTALLATION.
2. IRRIGATION DESIGN TO BE PREPARED BY IRRIGATION CONTRACTOR AND PROVIDED TO OWNER AND ARCHITECT FOR APPROVAL PRIOR TO INSTALLATION.
3. IRRIGATION CONNECTION TO BE COORDINATED BETWEEN THE CONTRACTOR AND ARCHITECT. INSTALL IRRIGATION METER AND BACKFLOW PREVENTION DEVICES AS REQUIRED BY THE CITY. SIZES TO BE DETERMINED BY IRRIGATION CONTRACTOR AND SHALL BE LISTED ON THE IRRIGATION PLANS.
4. IRRIGATION CONTRACTOR TO DETERMINE AVAILABLE WATER PRESSURE AND FLOW AT TAP LOCATIONS. IRRIGATION CONTRACTOR TO DESIGN IRRIGATION SYSTEM BASED ON AVAILABLE WATER PRESSURE. IRRIGATION CONTRACTOR TO PROVIDE ALL DESIGN CALCULATIONS FOR REVIEW BY THE OWNER OR ARCHITECT INCLUDING FLOW RATES AND PRESSURE LOSS CALCULATIONS OF THE SYSTEM.
5. THE IRRIGATION CONTROLLER TO BE INSTALLED IN A LOCKABLE, METAL PEDESTAL IN A LOCATION APPROVED BY THE OWNER OR ARCHITECT. THE IRRIGATION CONTROLLER SHALL BE MANUFACTURER'S SPECIFICATIONS. THE IRRIGATION CONTROLLER SHALL HAVE A MANUFACTURER'S FIVE (5) YEAR WARRANTY.
6. THE IRRIGATION SYSTEM DESIGN SHALL INCLUDE A WIRELESS RAIN SENSOR. INSTALL RAIN SENSOR IN LOCATION APPROVED BY THE OWNER AND PER MANUFACTURER'S SPECIFICATIONS. THE WIRELESS RAIN SENSOR SHALL BE MANUFACTURED BY HUNTER INDUSTRIES, RAIN BIRD CORPORATION, OR TORO.
7. IRRIGATION CONTROL VALVES ARE TO BE A PLASTIC GLOBE VALVE CONFIGURATION WITH PRESSURE RATING OF 15–150 PSI AND FLOW RANGE FROM 2 – 150 GPM. THE SOLENOID SHALL BE HEAVY DUTY ONE PIECE CONSTRUCTION. THE CONTROL VALVES SHALL HAVE THE ABILITY TO ADJUST AND REGULATE PRESSURE FROM 20–100 PSI. THE CONTROL VALVE SIZE SHALL BE 1", 1.5" AND 2" CONFIGURATIONS. THE CONTROL VALVES SHALL HAVE A MANUFACTURER'S THREE (3) YEAR WARRANTY.
8. TURF AREAS SHALL BE IRRIGATED USING TURF SPRAY HEADS. THE IRRIGATION SPRAY BODY SHALL BE UV–RESISTANT PLASTIC WITH A CO–MOLDED OR RUBBER SEALS AND HEAVY DUTY SPRING. THE SPRAY BODY SHALL HAVE BOTH A BOTTOM AND SIDE IN–LET FEATURE. SPRAY NOZZLES SHALL BE VARIABLE ARC SPRAY NOZZLES. THE IRRIGATION DESIGN SHALL BE PREPARED TO PROVIDE A 50% SPACING OR HEAD TO HEAD LAYOUT. THE SPRAY BODIES AND NOZZLES SHALL BE MANUFACTURED BY HUNTER INDUSTRIES, TORO, OR RAIN BIRD CORPORATION AND SHALL HAVE A MANUFACTURER'S FIVE (5) YEAR WARRANTY.
9. ALL IRRIGATION MAINLINE PIPES SHALL BE PVC CLASS 200 IPS PLASTIC PIPE. ALL PIPE SHALL BE SIZED NOT TO EXCEED A VELOCITY OF FIVE (5) FEET PER SECOND.
10. ALL IRRIGATION LATERAL PIPES SHALL BE POLYETHYLENE PIPE. ALL PIPE SHALL BE SIZED NOT TO EXCEED A VELOCITY OF FIVE (5) FEET PER SECOND.
11. ALL IRRIGATION PIPE OR WIRE SHALL BE INSTALLED IN SLEEVES BELOW ANY PAVEMENT OR HARD SURFACE. ALL IRRIGATION SLEEVES SHALL BE PVC SCHEDULE 40. ALL SLEEVES ARE TO EXTEND 24" BEYOND AND SIZED A MINIMUM OF TWO (2) TIMES THE DIAMETER OF THE MAINLINE OR LATERAL.
12. IRRIGATION CONTRACTOR SHALL SUPPLY MAINTENANCE AND OPERATION MANUALS AND A SCALED AS–BUILT DRAWING OF THE SYSTEM TO THE OWNER. IRRIGATION CONTRACTOR SHALL DEMONSTRATE THE PROGRAMMING OF THE CONTROLLER FOR THE OWNER.
13. FINISH GRADES ESTABLISHED PRIOR TO INSTALLATION OF IRRIGATION SYSTEM SHALL BE MAINTAINED AND ANY EXCESS SOIL SHALL EITHER BE WASTED ON SITE AT THE DISCRETION OF THE GENERAL CONTRACTOR OR DISPOSED OFF–SITE. ALL CONSTRUCTION DEBRIS FROM IRRIGATION OPERATIONS SHALL BE DISPOSED OFF–SITE.

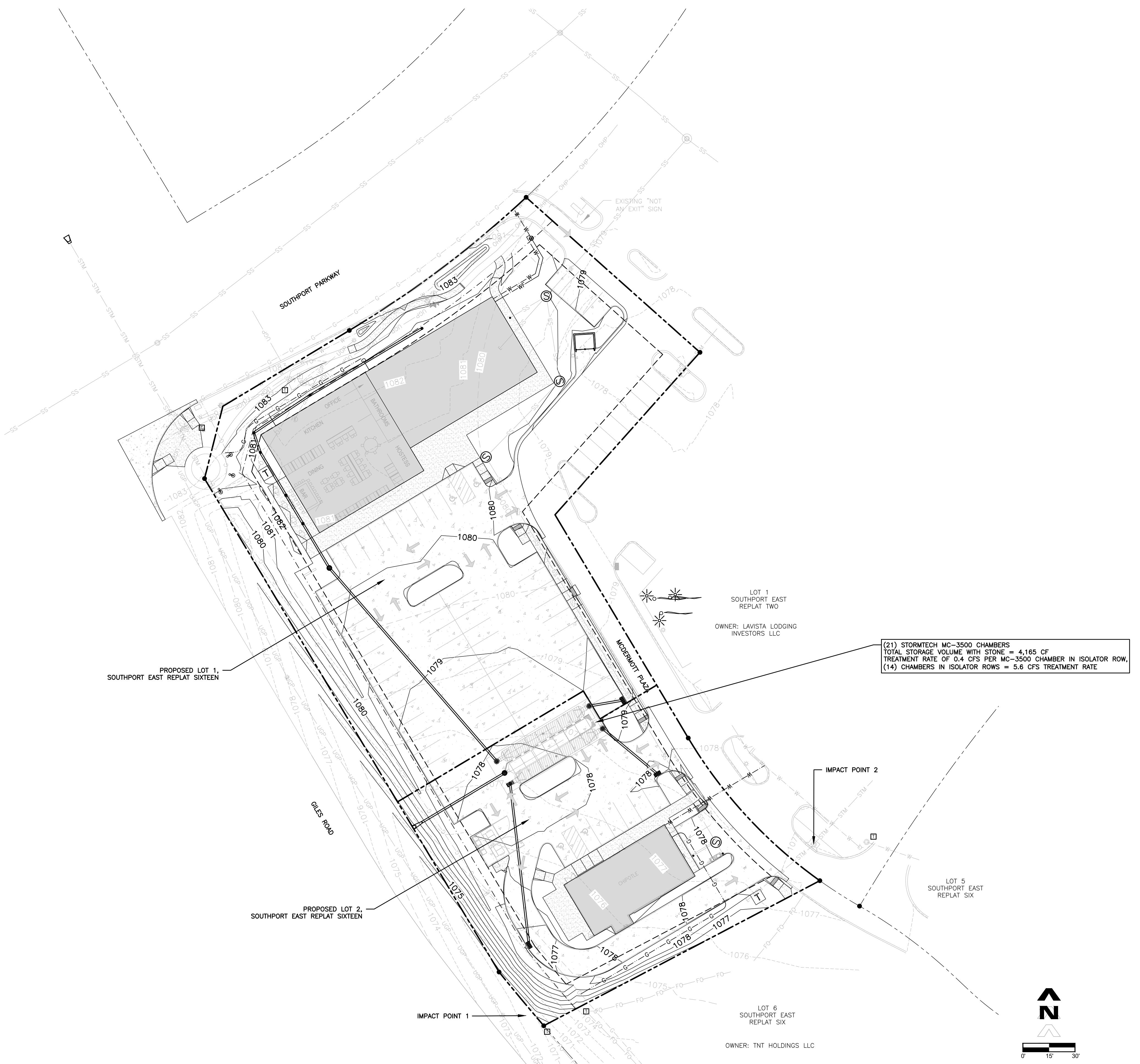
Position Paper

Drawn By: GTN Reviewed By: JAD
Job No : 2259-151 Date: 05-06-24

Sheet Title

Ch 4 N 1

C4.0



DESCRIPTION	PEAK RUN-OFF FROM PROJECT SITE	
IMPACT POINT 1	2-YR (CFS)	10-YR (CFS)
PRE-DEVELOPMENT	1.721	3.886
POST-DEVELOPMENT (W/ DETENTION)	1.537	4.792
IMPACT POINT 2	2-YR (CFS)	10-YR (CFS)
PRE-DEVELOPMENT	1.002	1.956
POST-DEVELOPMENT (W/O DETENTION)	0.969	1.707

Revision Date

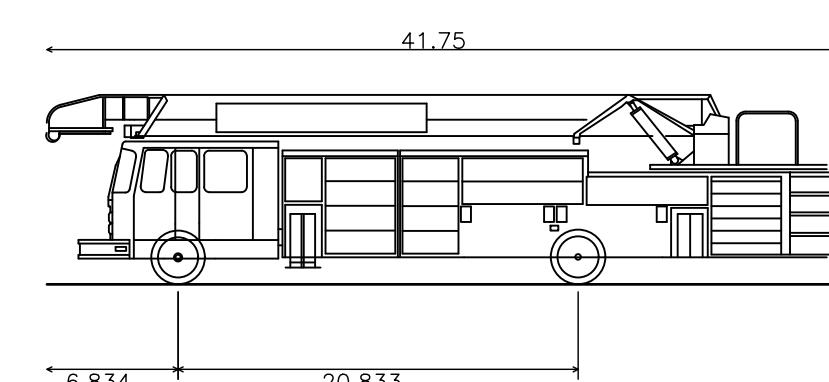
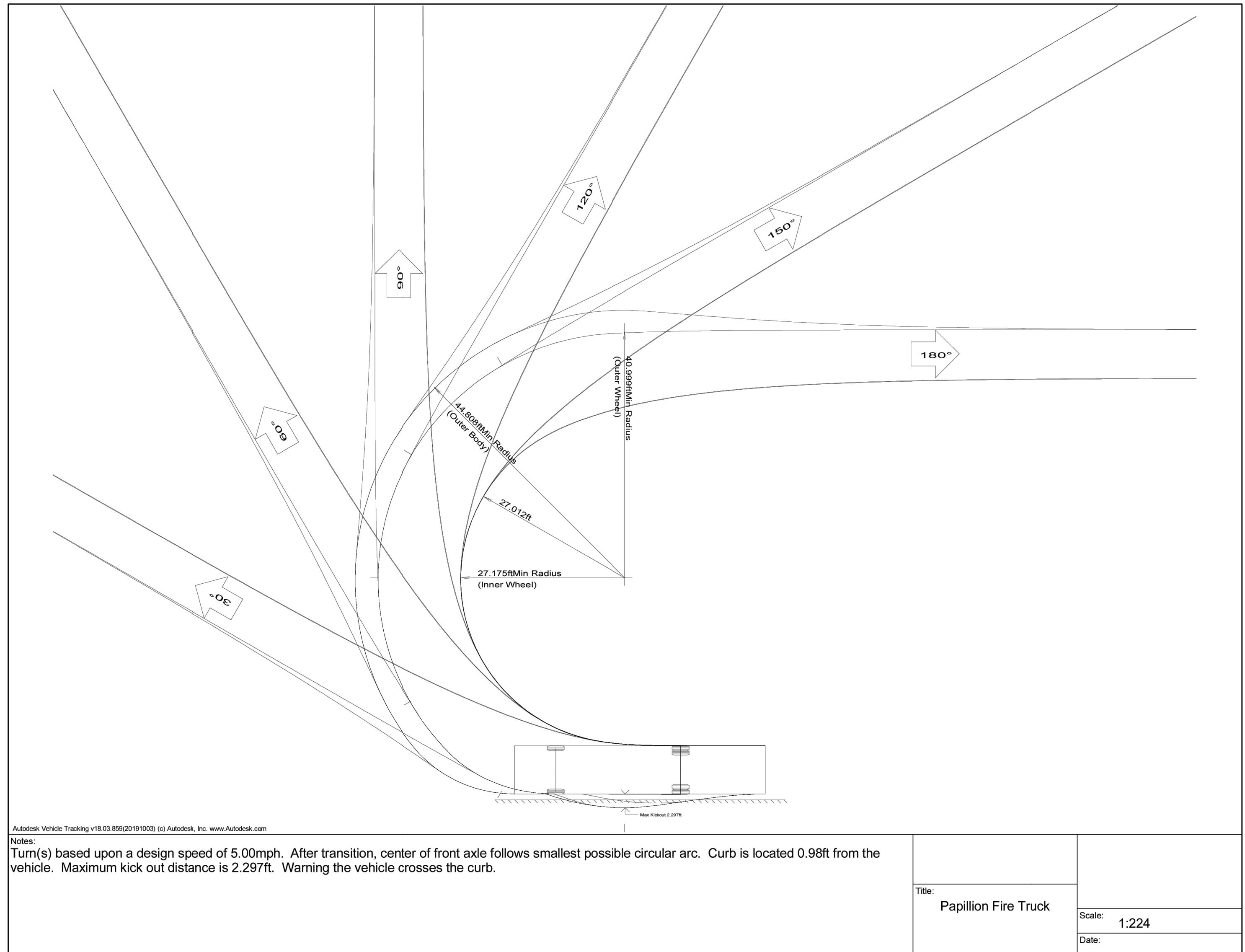
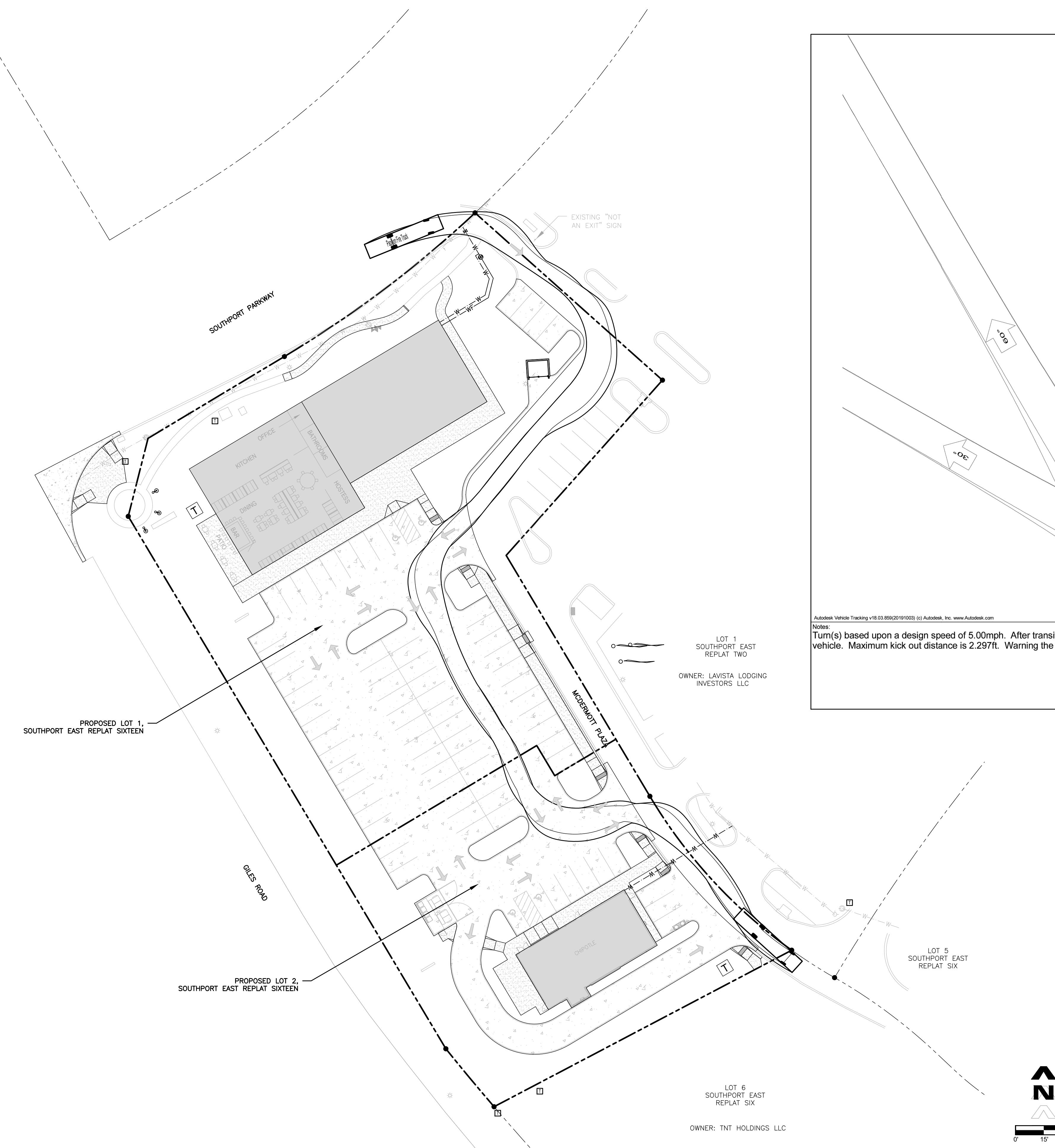
Drawn By: GTN Reviewed By: JAD
Job No.: 2259-151 Date: 05-06-24

Sheet Title

PCSMP & Drainage Plan

Sheet Number

C5.0



Papillion Fire Truck	
Overall Length	41.750ft
Overall Width	8.000ft
Overall Body Height	9.864ft
Min Body Ground Clearance	0.875ft
Max Track Width	8.142ft
Lock-to-lock time	5.00s
Max Wheel Angle	37.00°

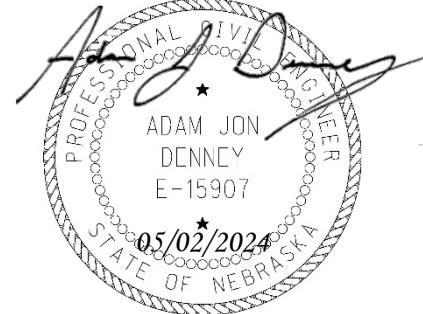
MEMORANDUM

TO: Joe Dethlefs, PE
 Thompson, Dreessen & Dorner, Inc.

FROM: Adam Denney, PE, PTOE & Peyton Weiss, EI
 Felsburg Holt & Ullevig

DATE: May 2, 2024

SUBJECT: Southport Lot 2 Replat 2 Update Trip Gen Memo
 FHU Reference No. 122100-11



Project Background

Felsburg Holt & Ullevig (FHU) has completed a trip generation comparison for the proposed Southport Lot 2 Replat 2 development located in La Vista, NE, just east of Giles Road and south of Southport Parkway. The Southport Lot 2 Replat 2 Update Traffic Impact Analysis (TIA) was completed in April of 2022. From that study, the development was anticipated to generate approximately 1,960 daily weekday vehicle-trips. This includes 184 vehicle-trips during the AM peak hour, and 153 vehicle-trips during the PM peak hour.

Since the completion of that study, proposed land uses have changed, reducing the number of trips from the site. This memorandum evaluates land use changes and compares them to the previously identified uses assumed in the April 2022 TIA.

Proposed Changes

The original site plan (previous uses) included a 4,200 square foot (SF) quick-service breakfast restaurant, a 1,500 SF retail business, a 2,255 SF fast-food restaurant with drive-through, and a 664 SF coffee shop with drive-through.

In the updated site plan (proposed uses), the coffee shop was no longer included, the quick-service breakfast restaurant was increased to 4,485 SF, the retail business was increased to 4,200 SF, and the fast-food restaurant with drive-through was increased to 2,325 SF.

Site Trip Generation

Trip generation average rates from the Institute of Transportation Engineers' (ITE) Trip Generation Manual, Eleventh Edition, 2021, were utilized to estimate the traffic generated by the updated site. **Table I** summarizes the estimated vehicle trips that the proposed development would generate, which is anticipated to generate 1,805 daily weekday vehicle trips. This includes 108 vehicle trips during the AM peak hour, and 144 vehicle trips during the PM peak hour.

Table I. Site Trip Generation – Proposed Uses

ITE Code	Land Use Description	Daily	AM Peak Hour			PM Peak Hour		
		Total	In	Out	Total	In	Out	Total
932	High-Turnover (Sit-Down) Restaurant	481	24	19	43	25	16	41
814	Variety Store	267	7	6	13	14	14	28
934	Fast-Food Restaurant with Drive-Through Window (Chipotle)	1,057	27	25	52	39	36	75
Total Vehicles		1,805	58	50	108	78	66	144

**Average Rates Utilized KSF= 1,000 Square Feet

May 2, 2024

Southport Lot 2 Replat 2 Update Trip Gen Memo

Page 2

Table 2 summarizes the trip generation for the previous uses assumed as part of the April 2022 TIA, which were anticipated to generate approximately 1,960 daily weekday vehicle-trips. This includes 184 vehicle-trips during the AM peak hour, and 153 vehicle-trips during the PM peak hour.

Table 2. Site Trip Generation - Previous Uses

ITE Code	Land Use Description	Daily	AM Peak Hour			PM Peak Hour		
		Total	In	Out	Total	In	Out	Total
932	High-Turnover (Sit-Down) Restaurant	450	22	18	40	23	15	38
814	Variety Store	95	3	2	5	5	5	10
938	Coffee/Donut Shop with Drive-Through Window and No Indoor Seating	358	44	45	89	15	15	30
934	Fast-Food Restaurant with Drive-Through Window	1,057	26	25	50	39	36	75
Total Vehicles		1,960	95	90	184	82	71	153

**Average Rates Utilized KSF= 1,000 Square Feet

Table 3 provides a comparison of the previous and proposed uses for the development site. With the proposed layout, daily trips and peak hour trips are all expected to be less than those of the previous layout.

Table 3. Site Trip Generation Comparison

Scenario	Daily	AM Peak Hour			PM Peak Hour		
		In	Out	Total	In	Out	Total
Proposed Uses (2024 Update)	1,805	58	50	108	78	66	144
Previous Uses (April 2022 TIA)	1,960	95	90	184	82	71	153
Change in Trips = Proposed Uses - Previous Uses	-155	-37	-40	-76	-4	-5	-9

Summary and Recommendations

Based on the trip generation analysis results, no significant impact to traffic conditions is anticipated with the proposed changes in land uses, and an update to the Southport Lot 2 Replat 2 Update Traffic Impact Analysis is not required.

Attachments

Trip Generation Tables

ITE Trip Generation Estimates - 11th Edition

Southport Scooters TIA

La Vista, NE

May 2, 2024



Trip Generation Comparison - Proposed Uses

Lot	ITE Code	Land Use Description	Size	Unit	Daily		AM Peak Hour			PM Peak Hour		
					TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL	
1	932	High-Turnover (Sit-Down) Restaurant	4.485	KSF	481	24	19	43	25	16	41	
1	814	Variety Store	4.2	KSF	267	7	6	13	14	14	28	
2	934	Fast-Food Restaurant with Drive-Through Window (Chipotle)	2.325	KSF	1,057	27	25	52	39	36	75	
Total Vehicles					1,805	58	50	108	78	66	144	

**Average Rates Utilized KSF= 1,000 Square Feet

Trip Generation Comparison - Previous Uses

Lot	ITE Code	Land Use Description	Size	Unit	Daily		AM Peak Hour			PM Peak Hour		
					TOTAL	IN	OUT	TOTAL	IN	OUT	TOTAL	
1	932	High-Turnover (Sit-Down) Restaurant	4.2	KSF	450	22	18	40	23	15	38	
1	814	Variety Store	1.5	KSF	95	3	2	5	5	5	10	
1	938	Coffee/Donut Shop with Drive-Through Window and No Indoor Seating	2.0	DTL	358	44	45	89	15	15	30	
2	934	Fast-Food Restaurant with Drive-Through Window	2.255	KSF	1,057	26	25	50	39	36	75	
Total Vehicles					1,960	95	90	184	82	71	153	

**Average Rates Utilized KSF= 1,000 Square Feet DTL=Drive-Through Lanes

Trip Generation Comparison - Change

Scenario	Daily	AM Peak Hour			PM Peak Hour		
		TOTAL	IN	OUT	TOTAL	IN	OUT
Proposed Uses	1,805	58	50	108	78	66	144
Previous Uses	1,960	95	90	184	82	71	153
Change = Proposed Uses - Previous Uses	-155	-37	-40	-76	-4	-5	-9